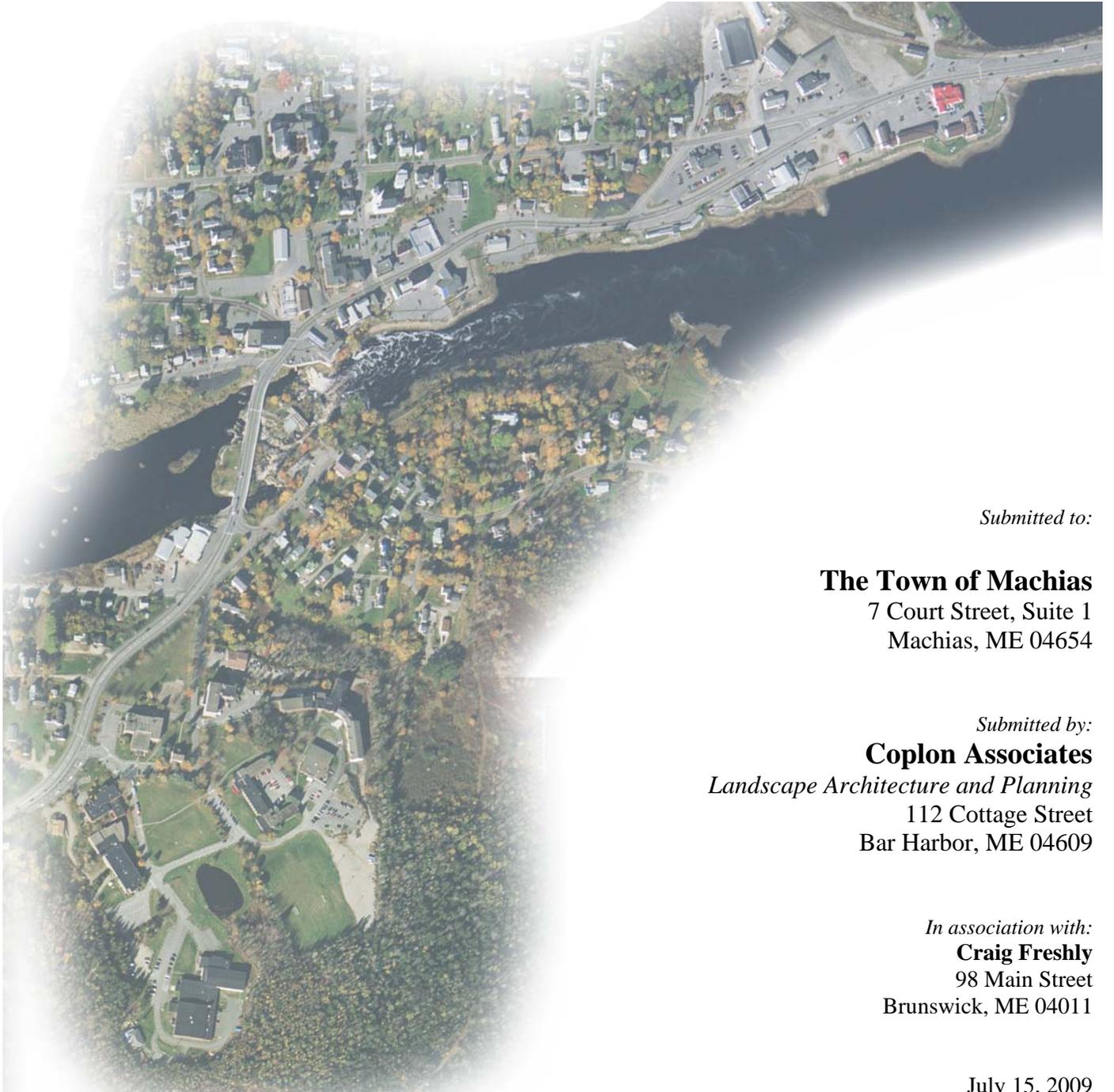


# *Machias Downtown and Riverfront Master Plan*

Machias, Maine



*Submitted to:*

**The Town of Machias**

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*Appendix A – Conceptual Master Plan Cost Estimate*

## I. INTRODUCTION

The Town of Machias is initiating a significant planning effort directed at revitalizing its downtown and riverfront area. The study, focused on the Route 1 downtown corridor extending from College Hill to the causeway, will support ongoing efforts to create a thriving, multiuse center for commercial and residential activity, open space and recreation. This effort should create far reaching benefits for the social and economic life of Machias.

Machias has long served as a commercial, social and educational center for Downeast Maine. Incorporated in 1784, the rich history of the town has been tied to geography—namely its location along the Machias River and the coastal turnpike. The past has an important role to play in Machias' future—from the town's significance in the Revolutionary War to the highlighting of important commercial (water-powered mills) and social (brick and granite buildings of the Baptist, Methodist, Universalist and Catholic churches) institutions characteristic of coastal New England settlements. Together with its ongoing role as a regional commercial center and its beautiful riverfront setting, Machias has substantial resources on which to draw to ensure economic stability and long-term viability.

This report contains recommendations for improvements to downtown Machias with particular focus on the areas adjacent to the Machias River. The plan sets forth ideas for streetscape and open space improvements that will enhance the appearance, function and visibility of the downtown area. Opportunities are present for improved pedestrian access and circulation, better connectivity of the open spaces along the river, upgraded and expanded downtown parking. New downtown development featured in the plan is directed toward ensuring long-term viability for downtown Machias. To address connectivity of the components within the corridor that define or contribute to the downtown, the study area extends northward along Route 1 from the campus of the University of Maine at Machias to the causeway and boat launch area at the north end of town and includes the area around the Burnham Tavern.

These recommendations set forth in the study are based on the input of Machias citizens and business owners. Through a series of meetings and public workshops, a number of key planning principles were developed to guide the downtown riverfront master plan. These recommendations included:

- Create new opportunities for parks and open space, centered on the Machias River.
- Bring to light the history of Machias by highlighting the river and the historic buildings and landmarks.
- Expand the pedestrian system to include a walkway along the river and a nature walk.
- Create better linkages between the university and its surrounding neighborhoods to the downtown.
- Create and improve public parking areas.
- Enhance the appearance of the downtown with streetscape improvements, including street trees and signage.
- Provide a pedestrian-only connection between the east and west sides of the river.

- Create an opportunity for the redevelopment or reuse of the old Ford Garage.

The master plan described in the report incorporates these ideas into a comprehensive series of recommendations that address open space and development opportunities, circulation, parking and streetscape improvements. The report is divided into several sections that describe the key elements of the master plan in detail. These elements include new parks and picnic areas linked by a network of pathways along the river and connections to the pedestrian improvements along the Route 1 corridor, parking improvements that include elimination of mid-block bump-outs in the downtown, realigning parking areas and reclaiming excess pavement for green space. The historic qualities of the downtown, including several structures listed on the National Register of Historic Places, can serve as the basis for a “museum in the streets” program that, as part of a larger promotional package, can attract visitors from the steady stream of traffic flowing through downtown.

## II. EXISTING CONDITIONS

Downtown Machias offers a unique combination of business activity and recreational opportunities. The primary natural resource in town is the Machias River, a historic riverway that has played a central role in the history and development of the community. Currently the relationship between the town and the river is not fully realized as visual connections to the Machias River are limited and pedestrian access to and along the river is greatly restricted. Much of the frontage along the river is privately owned and characterized by conditions ranging from narrow rip-rap embankments to large open parking areas. Public open space along the river is limited, with the most significant areas being the two public parks at either end of town—Bad Little Falls Park at the south and the boat launch area to the north. Located off the Route 1 Bridge, Bad Little Falls Park is a remarkable resource, offering close contact to the dramatic falls and to the remnants of historic infrastructure. The park includes a scenic platform, pedestrian bridge, garden and picnic area. Many of the park's features are in poor repair and accessibility and safety improvements are needed. The boat launch and picnic area at the north end of town are in good repair and offer fine views down the Machias River.

Machias has a rich history dating to colonial times. A number of landmark buildings including Burnham Tavern, Porter Memorial Library, Washington County Jail and Court House, Center Street Congregational Church and the Machias Railroad Station trace the history of the community and are listed on the National Register of Historic Places. Within the study area, there are a number of buildings undergoing or in need of renovation or facade improvements. The reuse of the Ford Garage building or redevelopment of the site has been limited by potential asbestos and soil contamination issues.

Located along U.S. Route 1, the downtown area experiences a significant volume of traffic, particularly in the summer season. Parking remains an issue with downtown merchants due to the limited amount of on-street parking and the lack of visibility of municipal parking lots. The curbside bump-outs in the heart of the downtown create obstacles for parking and are an aggravation for business owners, residents, visitors and public works crews.

Sidewalks are in generally good repair, with the exception of several curb ramps, which currently do not meet Americans with Disabilities Act (ADA) standards.

The northern gateway to town is characterized by a number of restaurants and businesses located within largely undifferentiated parking areas, creating a striking contrast between the tightly spaced buildings of the downtown core and the more dispersed development adjacent to the causeway. This contrast serves to visually disconnect the north end of town from the downtown, and the tenuous pedestrian connections reinforce the separation of the adjoining areas. The traffic patterns at the north end of town also contribute to the area's lack of definition. The angle and multiple connections at the intersection of Route 1 and Court Street create a confusing traffic flow for those approaching town from the north.

**LEGEND**

	BUILDINGS		75' SHORELINE SETBACK
	PAVED AREAS		VIEWING CORRIDORS
	PUBLIC OPEN SPACE		AREAS OF CONFLICT
	PRIVATE OPEN SPACE		GATEWAYS
	RIVER		PEDESTRIAN CONNECTION?

CALAIS BRANCH - RAIL BED CONVERSION TO  
DOWNEAST SUNRISE TRAIL,  
REGIONAL RECREATIONAL  
TRAIL CONNECTION

BOX CAR  
OLD RAIL STATION - NEW  
COMMERCIAL USE?  
TRAIL HEAD PARCEL

NORTHERN GATEWAY TO TOWN  
DOMINATED BY PARKING LOTS

BOAT  
LANDING  
VIEWS DOWN  
AND ACROSS  
THE RIVER

LANDMARK STEEPLE

DOWNTOWN CORE

OPPORTUNITY FOR PEDESTRIAN  
CONNECTIONS ALONG RIVER?

MACHIAS RIVER

VIEWS DOWN  
RIVER

STEEP SLOPE

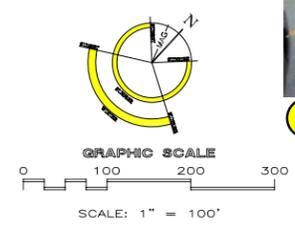
POOR ACCESSIBILITY TO  
BAD LITTLE FALLS PARK

VIEWS  
UP RIVER

SOUTHERN GATEWAY TO DOWNTOWN  
MACHIAS, GREAT VIEWS/PERSPECTIVE  
OF THE DOWNTOWN

UNIVERSITY OF MAINE  
AT MACHIAS

SOUTHERN GATEWAY



1



4



3



2



7



6



5

**MACHIAS DOWNTOWN AND RIVERFRONT MASTER PLAN - MACHIAS, MAINE**  
**EXISTING CONDITIONS AND SITE ANALYSIS**

### III. EXECUTIVE SUMMARY

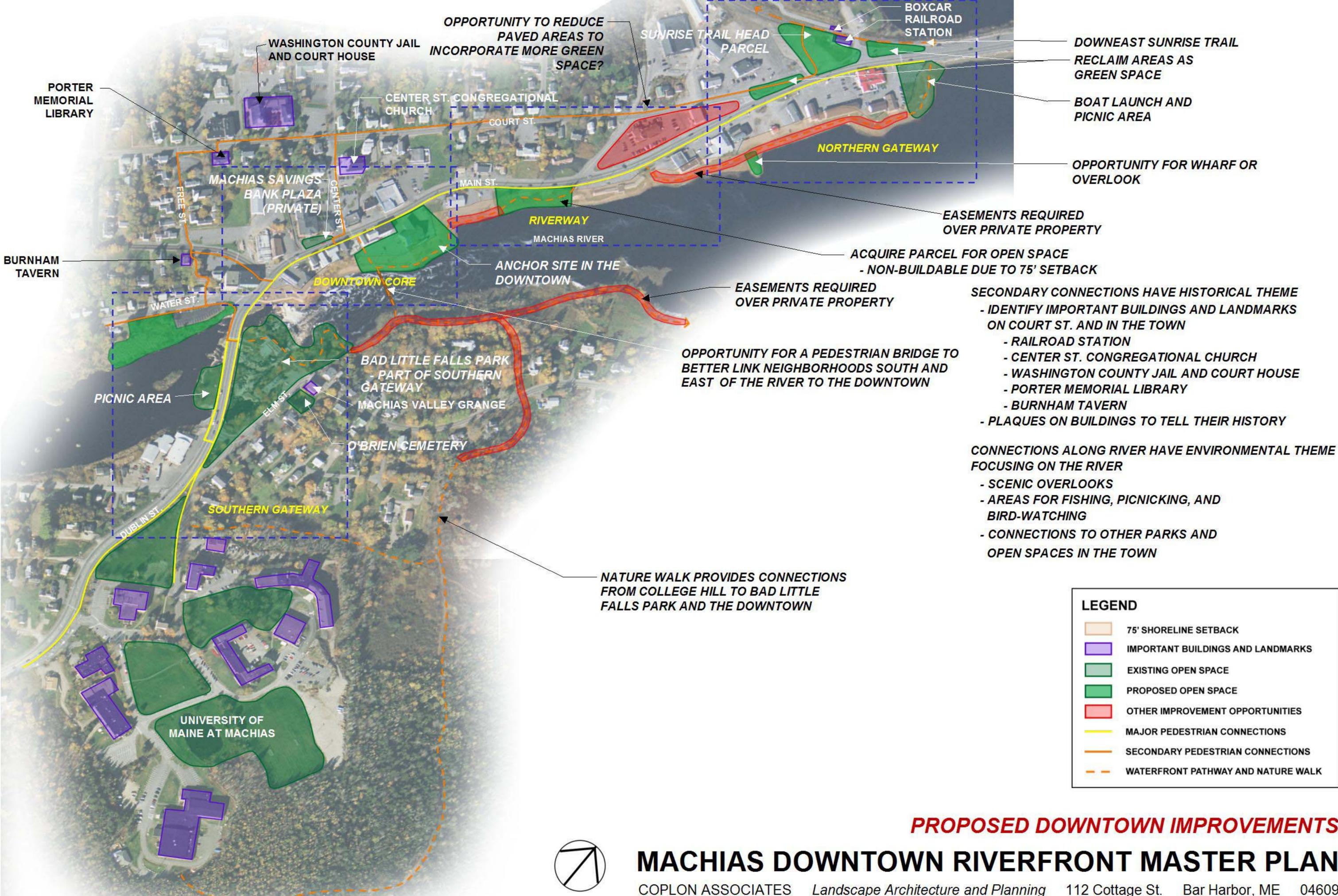
The recommended plan for downtown Machias creates additional open spaces and new routes for pedestrian circulation. The park system is improved, expanded and connected via an upgraded pedestrian circulation system. A new continuous recreational trail along the Machias River is proposed to connect the open spaces, and, with a new connection across the Machias River, the trail system can be enlarged and expanded to offer several recreational loops. A historic walk to create a “museum in the streets” is proposed along Court Street, Free Street and Center Street, identifying and interpreting important buildings and landmarks in the downtown area.

The master plan details the recommended improvements in four major sections of town:

- **Southern Gateway** – Extends from the bottom of the university hill to the intersection of Main Street (Route 192) and Route 1, encompassing Bad Little Falls Park and the Burnham Tavern.
- **Downtown Core** – Extends from Mobil Station just over the Route 1 Bridge to Bar Harbor Bank & Trust.
- **Riverway** – Connects the downtown core to the northern end of town, extending from Bar Harbor Bank & Trust to the area past the Irving Station and includes the vacant parcel next to the bank.
- **Northern Gateway** – Area from the intersection of Route 1 and Court Street to the causeway past Helen’s Restaurant.

An important recommendation of the plan is to create a feature or anchor in the downtown that will attract people from the university and surrounding neighborhoods. The renovation of the old Ford Garage into a community center or the creation of a large in-town public space in that location would serve this purpose. The development of a pedestrian bridge from a new riverside park near the garage to the east side of the river would greatly improve pedestrian connections from the surrounding neighborhoods and university to the downtown.

Streetscape improvements are crucial for the revitalization of the downtown to improve its appearance and appeal. Proposed improvements include new light fixtures in the Downtown Core, new street furnishings including benches and picnic tables, street trees along Route 1 and a comprehensive graphic package for direction, information and interpretation.



WASHINGTON COUNTY JAIL AND COURT HOUSE

OPPORTUNITY TO REDUCE PAVED AREAS TO INCORPORATE MORE GREEN SPACE?

SUNRISE TRAIL HEAD PARCEL

BOXCAR RAILROAD STATION

DOWNEAST SUNRISE TRAIL

RECLAIM AREAS AS GREEN SPACE

BOAT LAUNCH AND PICNIC AREA

OPPORTUNITY FOR WHARF OR OVERLOOK

PORTER MEMORIAL LIBRARY

CENTER ST. CONGREGATIONAL CHURCH

COURT ST.

NORTHERN GATEWAY

EASEMENTS REQUIRED OVER PRIVATE PROPERTY

MACHIAS SAVINGS BANK PLAZA (PRIVATE)

MAIN ST.

RIVERWAY

MACHIAS RIVER

ACQUIRE PARCEL FOR OPEN SPACE - NON-BUILDABLE DUE TO 75' SETBACK

BURNHAM TAVERN

DOWNTOWN CORE

ANCHOR SITE IN THE DOWNTOWN

EASEMENTS REQUIRED OVER PRIVATE PROPERTY

SECONDARY CONNECTIONS HAVE HISTORICAL THEME

- IDENTIFY IMPORTANT BUILDINGS AND LANDMARKS ON COURT ST. AND IN THE TOWN
- RAILROAD STATION
- CENTER ST. CONGREGATIONAL CHURCH
- WASHINGTON COUNTY JAIL AND COURT HOUSE
- PORTER MEMORIAL LIBRARY
- BURNHAM TAVERN
- PLAQUES ON BUILDINGS TO TELL THEIR HISTORY

WATER ST.

OPPORTUNITY FOR A PEDESTRIAN BRIDGE TO BETTER LINK NEIGHBORHOODS SOUTH AND EAST OF THE RIVER TO THE DOWNTOWN

CONNECTIONS ALONG RIVER HAVE ENVIRONMENTAL THEME

- FOCUSING ON THE RIVER
- SCENIC OVERLOOKS
- AREAS FOR FISHING, PICNICKING, AND BIRD-WATCHING
- CONNECTIONS TO OTHER PARKS AND OPEN SPACES IN THE TOWN

PICNIC AREA

BAD LITTLE FALLS PARK - PART OF SOUTHERN GATEWAY

MACHIAS VALLEY GRANGE

O'BRIEN CEMETERY

NATURE WALK PROVIDES CONNECTIONS FROM COLLEGE HILL TO BAD LITTLE FALLS PARK AND THE DOWNTOWN

SOUTHERN GATEWAY

ELM ST.

DUBLIN ST.

UNIVERSITY OF MAINE AT MACHIAS

**LEGEND**

- 75' SHORELINE SETBACK
- IMPORTANT BUILDINGS AND LANDMARKS
- EXISTING OPEN SPACE
- PROPOSED OPEN SPACE
- OTHER IMPROVEMENT OPPORTUNITIES
- MAJOR PEDESTRIAN CONNECTIONS
- SECONDARY PEDESTRIAN CONNECTIONS
- WATERFRONT PATHWAY AND NATURE WALK

**PROPOSED DOWNTOWN IMPROVEMENTS**

**MACHIAS DOWNTOWN RIVERFRONT MASTER PLAN**



## **IV. DOWNTOWN RIVERFRONT MASTER PLAN**

Recommendations for improving the function, appearance and appeal of Machias' downtown are focused on refinements to several key areas and the pedestrian system that connects them. These areas, as described below, include the Southern Gateway, Downtown Core, Riverway and Northern Gateway

### **A. Pedestrian Circulation**

Expansion and enhancement of pedestrian circulation is critical to ensuring the integrity and accessibility of the downtown area. The plan envisions a pedestrian circulation hierarchy that includes upgrades to the Route 1 sidewalk, improving connections to and along the river and linking the downtown to the campus and the residential area east of the river.

The primary pedestrian system for the downtown area is the existing sidewalk along Route 1 from the university to the boat launch at the north end of town. This walkway provides a continuous connection and is generally in good repair. As illustrated, the master plan upgrades to curb ramps, crosswalks and demarcation of pedestrian zones across parking aprons will allow for improved accessibility, visibility and safety for pedestrians. Cast-in-place concrete is the preferred sidewalk material and a minimum sidewalk width of 4 feet should be maintained.

Within the downtown area, a secondary system of sidewalks along Court Street, Free Street and Center Street adjacent to Route 1 provide convenient pedestrian access to a number of Machias' historic structures. Through a "museum in the streets" concept, these buildings can be linked by appropriate signage, allowing for self-guided interpretive walking tours of downtown. From the south, the walk could start at Bad Little Falls Park overlooking the falls, continue to the Burnham Tavern and on to the Porter Memorial Library, Washington County Jail and Court House and the Center Street Congregational Church, terminating at the boxcar and old railroad station across from Helen's Restaurant. A clear, consistent and well-designed sign system and graphic vocabulary will be important to identifying the "historic trail" and providing connections between landmarks.

The plan envisions a new walkway along the river, which would provide opportunities for passive recreation and visual access to the river within the downtown. This proposed walk links existing open spaces along the river extending from the site of the old Ford Garage to the picnic area next to the boat launch. As existing conditions, including available area and embankment material, vary along the route, the walk would consist of an aggregate trail 4 feet in width where space is available and a boardwalk with a handrail, in narrow areas with steep embankments. The majority of the proposed alignment for the trail crosses private property and will require easements from landowners.

An expanded nature walk is proposed on the east side of the river connecting Bad Little Falls Park and the University of Maine at Machias. Portions of the walk are already in place and require some additional clearing and widening. The walk should be surfaced with a soft material, such as wood chips. The riverwalk portions of the nature walk cross private property and will require securing access easements from landowners.

The plan proposes a new pedestrian bridge across the Machias River to link the downtown and riverwalk on the west side of the river with the nature trail and the university campus on the east side. At some point in the future, this bridge would connect the public parking area adjacent to the former Ford Garage on the west side of the river to the existing nature trail on the east. The bridge span would be approximately 220 feet and would allow for a pedestrian loop of approximately 3,600 feet (.68 miles) on both sides of the river. The bridge would offer dramatic up-river views to Bad Little Falls and down-river views to the boat launch and beyond.

**B. Southern Gateway**

The southern gateway to downtown Machias begins at the top of university hill where the view of the downtown area unfolds to northbound motorists and pedestrians. The gateway extends across the Route 1 Bridge to the intersection of Main Street (Route 192) and Route 1.

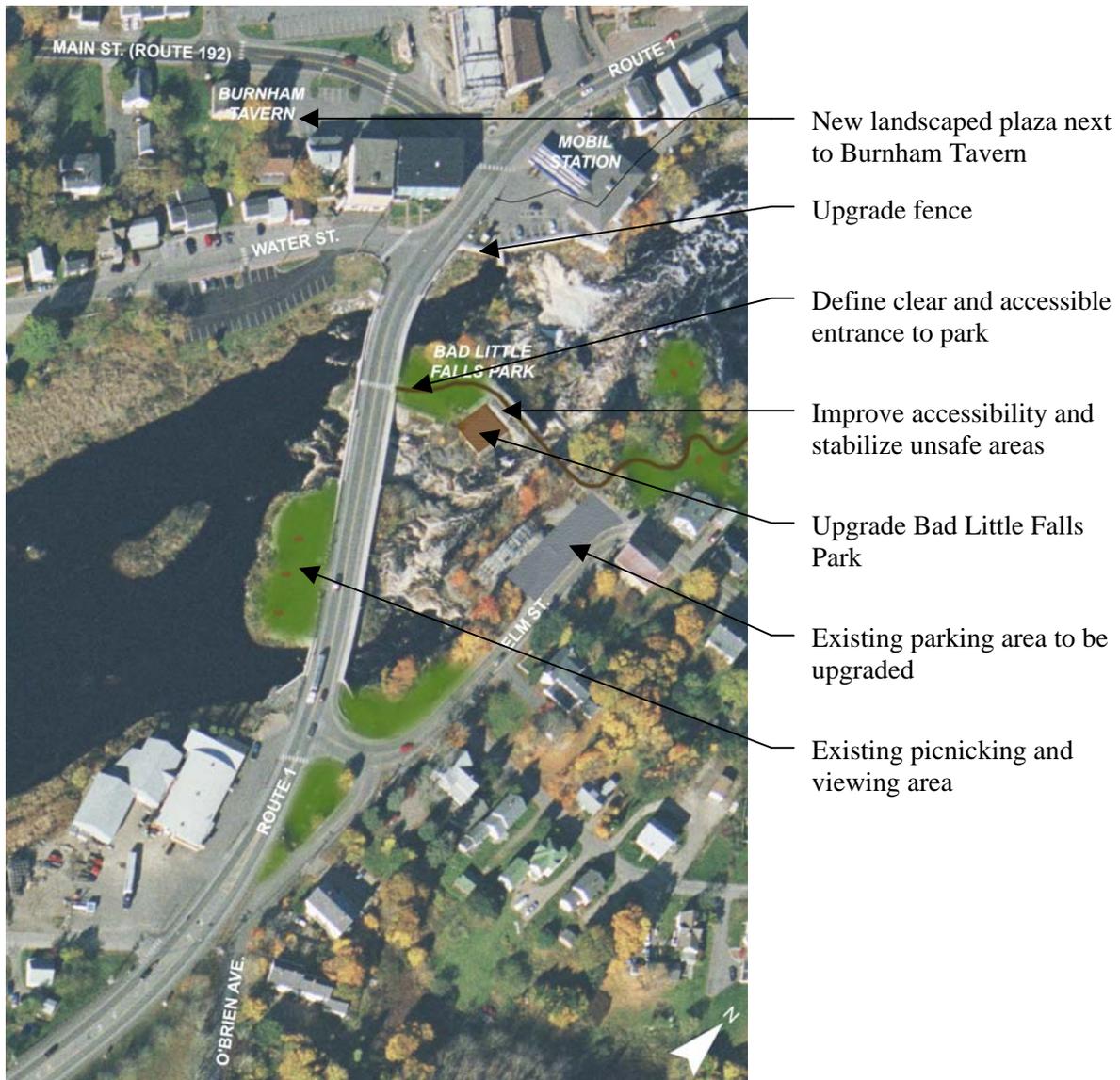


Figure 1. Conceptual plan for the Southern Gateway.

Several key issues should be addressed to make the southern gateway more visually appealing, functional and safe for pedestrians and motorists. These issues and recommended solutions are as follows:

*1. Clearly establish Southern Gateway*

The southern edge of the downtown can be more clearly demarcated by new identity, interpretative and directional signage, landscaping and open space improvements. A new gateway sign denoting Historic Downtown Machias and landscaping along the Route 1 corridor would help announce and define the entrance to the downtown.



Figure 2. Existing and proposed conditions at intersection of Routes 1 and 192 with the addition of street trees.

*2. The adjacency of southbound O’Brien Avenue eastward of Route 1 presents safety issues to northbound Route 1 traffic.*

Installation of trees and shrubs of evergreen plant material (salt-tolerant species such as Eastern white cedar, Blue spruce (figure 3)) in the median between the two roads would effectively be used to screen the view of O’Brien Avenue from Route 1 and focus attention on the downtown approach. Careful placement of a downtown gateway sign in this location could also serve to effectively shield the two roads from each other.

Provide screening between O’Brien Ave and Route 1  
New sign welcoming people to the historic downtown



Figure 3. Existing and proposed conditions entering the downtown from the south with screening from O’Brien Avenue and a new welcome sign.

*3. Access to and visibility of Bad Little Falls Park is limited. Portions of the park are in poor repair and present accessibility issues.*

Bad Little Falls Park is an important downtown resource. Improving the park’s entrance signature and upgrading walkways and facilities for universal access will make the park more appealing and allow it to better function as a primary downtown open space. Development of a new entrance pergola (figure 4) will help identify the park from Route 1. Upgrades to the walkway and viewing platform, including surfacing and railings in conjunction with other landscape improvements, will greatly serve to make the park more attractive to visitors and residents. Upgraded pathways from the east side would allow accessible connection to the gravel parking area on Elm Street, which can be improved by paving and signage. Interpretive signage in conjunction with a self-guided “museum in the streets” tour identifying other downtown landmarks can celebrate the significance of the site.

New pergola to define entrance to Bad Little Falls Park



Figure 4. Existing and proposed conditions of the entrance to Bad Little Falls Park.

*4. The chain-link fence on the south side of the Mobil Station detracts from the entrance to downtown.*

Replacement of the chain-link fence by a simple, decorative metal fence (figure 5) would improve the appearance of the southern gateway to town.

New metal fence  
Consolidate signs to improve visibility  
New crosswalk



Figure 5. Existing and proposed conditions on west side of Route 1 bridge with new fence above the retaining wall and relocated signs.

### 5. Celebrate the historic Burnham Tavern

Located at the intersection of Main Street (Route 192) and Free Street, the Burnham Tavern is one of Machias' most significant historic buildings and is a museum displaying artifacts and memorabilia from the Revolutionary and Civil Wars. The building would serve as a primary destination for the "museum in the streets." If possible, purchasing and removing the adjacent building would allow for the development of a new landscaped plaza, contributing to the downtown open space system.



Figure 6. Existing and proposed conditions of the Burnham Tavern with the adjacent building removed to create a landscaped plaza.

### C. Downtown Core

Extending from the Mobil Station to Bar Harbor Bank & Trust, the downtown core is an attractive, vibrant place characterized by a dense arrangement of one-, two- and three-story buildings set close to the road. In locations where buildings have been removed, private open spaces have been developed. The area is well served by a network of pedestrian sidewalks that were improved approximately 20 years ago during a downtown revitalization project.



Figure 7. Conceptual plan for the Downtown Core.

Consolidate parking area to create green space along the river

Reuse or rebuild old Ford garage as part of new anchor site

New pedestrian bridge

Some of these improvements are aging or present accessibility issues and are candidates for redevelopment. Several of the key issues facing the downtown core include:

*1. Business owners and town staff have identified the midblock curb extensions on the east side of Route 1 as creating problems for parking and snowplowing, with the result being damage to street lights, curbing and paving. Existing fences and walls along the sidewalks can be improved to be more attractive.*

Removal of these “bump-outs” will allow for a realignment of parallel parking and the possible addition of three spaces along the east side of the road. Space must be left open in the parking lane for a crosswalk and curb ramp. The sidewalk is relatively narrow in this location (approx 6 feet wide), requiring the midblock streetlight (currently in the curb extension) to be removed, located within the sidewalk or adjacent to a building. A review of the street lighting should be conducted before removal of the curb extension to ensure the lighting solution for this location is compatible with overall downtown lighting strategies. As part of this midblock improvement, the chain-link fence that lines the sidewalk where a building once stood should be replaced with an attractive wood fence that maintains the building line. The concrete wall along the Bangor Savings Bank property on the west side of Route 1 should be resurfaced with a masonry veneer

or new concrete skim coat. The addition of street trees in strategic locations along Route 1 would improve the appearance and add to the appeal of the downtown.

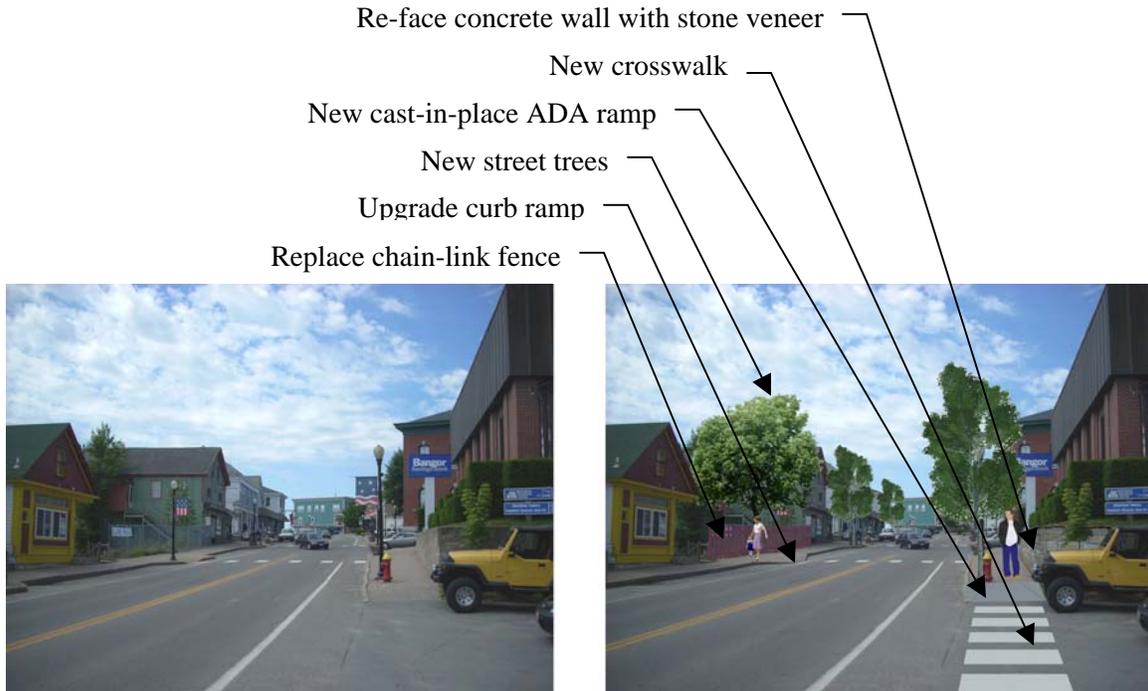


Figure 8. Existing and proposed conditions of downtown Main St. Downtown improvements include re-facing concrete wall in front of Bangor Savings Bank, replacing chain-link fence on opposite side of street, removing mid-block curb extensions, and adding street trees.

2. Due to the slope of the road in places, the grades of the sidewalk and curb ramps exceed current accessibility standards.

Where practicable, reconstruct sidewalk approaches to meet current accessibility standards. Curb ramp improvements should be included in all roadway/sidewalk and curb reconstruction projects.

3. Old Ford Garage is located in an important downtown location. The building is currently vacant and renovation or reuse of the site will likely require environmental remediation.

The building is in a key downtown location and reuse of the building or redevelopment of the site for commercial or public activity is important for economic vitality. One idea that has been promoted is the development of a cultural or community center to attract different age groups and to encourage the college community to spend more time downtown. To promote activity and compliment the future uses at this anchor site, the plan envisions the development of useable open space along the river. A more efficient reconfiguration of the existing parking lot behind the Ford Garage would allow the development of park land along the river. As envisioned, the reconfigured parking lot would contain approx 27 cars as oppose to the 37 it holds today, while allowing the recapture of approx 7,600 square feet (.17 acres) of green space. This new park, containing benches and tables for viewing and picnicking, would create a terminus for the riverway walkway and the new pedestrian bridge across the Machias River.



Figure 9. Existing and proposed conditions of anchor site in the downtown. Reconfiguring the existing parking area creates an opportunity for a new open space.

**D. Riverway**

The riverway extends along the west side of the Machias River from the Old Ford Garage to the boat launch site north of Helen’s Restaurant. The walkway improvements envisioned for this area allow for a better-defined and continuous pedestrian connection through the northern section of the downtown.



Figure 10. Conceptual plan of Riverway.

New river walk  
New open space with picnic and viewing areas next to Bar Harbor Bank & Trust

1. Improve connections between the downtown core and the causeway at the northern end of town.

The existing sidewalk along Route 1 is discontinuous and in some locations indistinguishable from the surrounding parking aprons and drives. Clearly demarcating the sidewalk will improve pedestrian safety and identify the importance of pedestrian connections in town. Where feasible, separating the sidewalk from the street edge with a landscape median would create a safer, more attractive pedestrian environment and allow for the planting of street trees. A continuous street tree planting extending through the downtown would help visually integrate the Route 1 corridor, provide a pleasant pedestrian environment and help modulate traffic speed.



Figure 11. Existing and proposed conditions looking toward the Northern Gateway with new street trees that provide separation between pedestrian and vehicular traffic.

2. Vacant parcel adjacent to Bar Harbor Bank & Trust

Within the shoreland zone where development is limited, this site offers a significant opportunity to create a public park/viewing area adjacent to the river. With benches and picnic tables and other landscaping, this site would create an important recreational node along the riverwalk. Bar Harbor Bank & Trust donated the parcel to the Town in 2008 and reclamation work is scheduled for the summer of 2009.



Figure 13. Existing and proposed conditions of new river walk connecting to pedestrian bridge at the anchor site.

### E. Northern Gateway

Whereas the downtown area has a closely-knit assemblage of buildings that define the road corridor, the northern gateway is largely the reverse, with buildings set back off the road and parking dominating the urban landscape. This area presents the most challenging and interesting opportunities to improve the appearance and function of the downtown and support regional recreational resources:



Figure 14. Conceptual plan for the Northern Gateway.

*1. There is an excess amount of paved area at the south end of the causeway, contributing to a lack of definition to the northern gateway.*

The area on the west side of the causeway dominated by gravel and broken pavement can be reclaimed as a green space to better define the entrance corridor to town and focus southbound attention on Route 1 and iconic commercial landmarks such as Helen's Restaurant. This open space is adjacent to the historic Railway Station and boxcar that serves as the trailhead for the Downeast Sunrise Trail. Together with potential improvements to landscape around these buildings, a significant new open space could be created as part of the northern entry to downtown Machias. The Downeast Sunrise Trail passes through the causeway open space, offering opportunities to improve awareness and visibility of the trail, interpret its history and significance and serve as an important regional recreational resource to create an important gateway feature to the downtown. In addition, the public boat launch across Route 1 provides easy access to the Machias River and Machias Bay.

*2. The intersection of Route 1 and Court Street is unattractive, poorly defined and confusing.*

Reworking of the Court Street/Route 1 intersection could greatly improve the traffic flow, safety and appearance of the north end of town. Removal of some pavement and the installation of landscaped traffic islands can better define traffic flow and turning movements while creating an attractive definition to the Route 1 corridor and Court Street intersection. A careful analysis of the intersection should be conducted by traffic engineers to determine recommended alignments, lane closures and road geometries.



Figure 15. Existing and proposed conditions at intersection of Route 1 and Court Street with enlarged landscaped island and new plantings to create a better-defined traffic pattern.

*3. Most of the parking in the area is private.*

Because Town-owned land is limited, in order to provide adequate parking in this area, the Town can explore working with abutting land owners to create parking that can serve both the Downeast Sunrise Trail and the boat launch.

## V. STREETSCAPE IMPROVEMENTS AND AMENITIES

The downtown revitalization calls for a number of streetscape improvements that include new lighting, benches, plantings and paving treatments. The vocabulary of selected site furnishings should be complimentary, providing a coordinated and attractive look to the downtown and Route 1 corridor.

- A. Lighting** – The existing decorative streetlights in downtown are in poor repair and consideration should be given to a comprehensive replacement with efficient dark sky friendly fixtures. Prior to purchasing new streetlights, it is recommended that a thorough lighting study be conducted to determine lighting needs and recommend fixture types, height locations, etc.
- B. Benches, Tables and Trash Receptacles** – Benches should be of traditional design with cast iron frame and wood slats, between 6 to 8 feet in length. Benches should be located in picnic and viewing areas, and under trees. They can also be located along the sidewalks, provided there is no interference with walking areas. Picnic tables should be of classic design made of either wood or metal. Tables should be 6 feet in length and be located in park areas. Trash receptacles should compliment the benches and tables in material and design. They should be durable, metal with a black powder-coated finish; a 35-gallon size is typical and should be located near intersections, benches and picnic areas. Locations and design of furnishings should be coordinated during the design phase of the project.



Figure 16. Examples of traditional site furnishings.

- C. Paving Treatments** – Removing the midblock bump-outs will require new curbing to be set. Curbing should be granite and selected to match the existing curbing. Sidewalk pavers should be reused as necessary to fill in any voids from construction. ADA ramps are to be provided at all crosswalk locations, and are to be cast-in-place concrete with detectable warnings. Painted crosswalks should highlight all pedestrian crossings on roadways.
- D. Street Trees** – Street trees are essential to creating an inviting and attractive downtown. The selection of appropriate species and proper planting methodologies are critical to ensuring the long-term vigor of street trees. Street trees will be chosen to either create a high canopy that extends over roads and surrounding buildings so as not to obscure signage and views, or to provide a narrow, tight canopy that creates a formal edge to the road. A narrow, tighter canopy is recommended in the denser parts of the downtown to avoid buildings and aerial utility lines. This will create separation between pedestrian and

vehicular traffic and strengthen the connections to the Northern and Southern Gateways.

Trees selected for planting in urban environments must be drought, heat and salt tolerant, as well as native species. Plant hardiness for Machias is USDA Zone 4 and several recommended species include:

- Street Trees
  - *Acer rubrum* – Red Maple Var. Armstrong is upright and narrow.
  - *Tilia cordata* – Littleleaf Linden
  
- Park Trees
  - *Acer rubrum* – Red Maple
  - *Acer saccharinum* – Silver Maple
  - *Betula nigra* – River Birch Var. Heritage
  - *Prunus serotina* – Black Cherry
  - *Quercus palustris* – Pin Oak not recommend for use as street tree
  - *Quercus rubra* – Red Oak
  - *Ulmus parviflora* – Lacebark Elm

Planting trees in the downtown area will require excavation for tree pits of sufficient size to allow root growth. In areas outside of the downtown core where conditions are not as constricting, it is recommended to plant trees in continuous landscaped bays to offer trees the greatest opportunity for growth and long-term health.

**E. Signage** –The ability to announce, identify and locate important destinations is essential to the success of a downtown. Signage plays a key role in this way-finding process. It is recommended that Machias develop a comprehensive signage program to identify its key downtown resources. This signage falls into three general categories: gateway, way-finding/interpretational and locational. Gateway signage should be placed at the Southern and Northern Gateways to welcome people to historic downtown Machias. These signs should be large enough to be visible to vehicles approaching downtown. Way-finding and interpretative signage are suitable to identify historic buildings and places and locational signage is suitable to tell the story of a specific site. There should be a unifying graphic vocabulary or theme to the signs, uniting the larger roadway entry signage to the smaller on-site interpretive signs. As has been done in many other communities, it is recommended that a graphic designer with experience in municipal signage and interpretation be retained to develop a comprehensive signage package for Machias.

**Appendix A**

**Machias Downtown and Riverfront Master Plan  
Conceptual Design Cost Estimate**

<b>Southern Gateway Improvements</b>						
Item	Quantity	Unit	Unit Cost	Item Total	Subtotal	Remarks
Purchase and Removal of Building		allow	\$165,000.00	\$165,000.00		
Building Demolition and Removal		allow	\$35,000.00	\$35,000.00		
Landscaped Plaza	3200	SF	\$35.00	\$112,000.00		stone plaza adjacent to Burnham Tavern
Signage	1	EA	\$600.00	\$600.00		Sign marking Southern Gateway
Fence Improvements	180	LF	\$75.00	\$13,500.00		Replace chain-link fence at Mobil Station
Evergreen Plantings	7	EA	\$600.00	\$4,200.00		Screening between Rt. 1 and O'Brien Ave.
Shrub Plantings	6	EA	\$75.00	\$450.00		Planting between Rt. 1 and O'Brien Ave.; shrubs behind fence at Mobil Station
<b>Subtotal</b>					<b>\$330,750.00</b>	
<b>Estimating Contingency (20%)</b>					<b>\$66,150.00</b>	
<b>Southern Gateway Total</b>					<b>\$396,900.00</b>	
<b>Bad Little Falls Park Improvements</b>						
Item	Quantity	Unit	Unit Cost	Item Total	Subtotal	Remarks
Trellis at Park Entrance		allow	\$15,000.00	\$15,000.00		Wood trellis to mark entrance to the park along Rt. 1, approx. 8' x 20' x 8'
Walk Improvements	300	LF	\$25.00	\$7,500.00		Resurface walk areas from Rt. 1 bridge to Elm St. with new aggregate and reset edging
Replace Railing	170	LF	\$75.00	\$12,750.00		Replace rail and repair concrete at spillway to meet ADA requirements; excludes existing wood stage area
<b>Subtotal</b>					<b>\$35,250.00</b>	
<b>Estimating Contingency (20%)</b>					<b>\$7,050.00</b>	
<b>Bad Little Falls Park Total</b>					<b>\$42,300.00</b>	
<b>Nature Walk Improvements</b>						
Item	Quantity	Unit	Unit Cost	Item Total	Subtotal	Remarks
Trail Improvements	6,580	LF	\$20.00	\$131,600.00		Assume 4' wide trail; clear and grub, base and surface trail from Bad Little Falls Park (Elm St. entrance) along eastern side of Machias River and circumnavigate UMM
Signage	7	EA	\$250.00	\$1,750.00		Way-finding interpretive signage
<b>Subtotal</b>					<b>\$133,350.00</b>	
<b>Estimating Contingency (20%)</b>					<b>\$26,670.00</b>	
<b>Nature Walk Total</b>					<b>\$160,020.00</b>	
<b>Anchor Site Improvements</b>						
Item	Quantity	Unit	Unit Cost	Item Total	Subtotal	Remarks
Pavement Removals		allow	\$2,600.00	\$2,600.00		Consolidate parking lot to maintain same number of spaces and remove excess pavement
Loam and Seed	34,500	SF	\$0.60	\$20,700.00		Loam and seed areas around old Ford Garage and where pavement was removed
Benches	4	EA	\$600.00	\$2,400.00		New benches along riverfront
Picnic Tables	6	EA	\$800.00	\$4,800.00		
Trash Receptacles	3	EA	\$300.00	\$900.00		
Evergreen Plantings	10	EA	\$600.00	\$6,000.00		Screening from adjacent properties
Shade Tree Plantings	12	EA	\$650.00	\$7,800.00		Specimen trees
<b>Subtotal</b>					<b>\$45,200.00</b>	
<b>Estimating Contingency (20%)</b>					<b>\$9,040.00</b>	
<b>Anchor Site Total</b>					<b>\$54,240.00</b>	
<b>Riverwalk Improvements</b>						
Item	Quantity	Unit	Unit Cost	Item Total	Subtotal	Remarks
Aggregate Walk Along River	1,850	LF	\$20.00	\$37,000.00		Assume 4' width
Boardwalk Along River	600	LF	\$150.00	\$90,000.00		Assume 4' width with handrail
Clearing and Grubbing		allow	\$2,500.00	\$2,500.00		Clear newly acquired parcel next to Bar Harbor Bank, leaving large trees
Loam and Seed	18,700	SF	\$0.60	\$11,220.00		Loam and seed parcel for new park area
Benches	5	EA	\$600.00	\$3,000.00		New benches along riverfront
Picnic Tables	6	EA	\$800.00	\$4,800.00		Tables in vacant parcel and by boat launch
Trash Receptacles	2	EA	\$300.00	\$600.00		
Evergreen Plantings	10	EA	\$600.00	\$6,000.00		Screening from adjacent properties
Shade Tree Plantings	10	EA	\$650.00	\$6,500.00		Specimen trees for park area
Signage	4	EA	\$250.00	\$1,000.00		Way-finding interpretive signage
<b>Subtotal</b>					<b>\$162,620.00</b>	
<b>Estimating Contingency (20%)</b>					<b>\$32,524.00</b>	
<b>Riverwalk Total</b>					<b>\$195,144.00</b>	

<b>Main Street Improvements</b>						
Item	Quantity	Unit	Unit Cost	Item Total	Subtotal	Remarks
Demolition and Removal of Mid-block Bump Outs		allow	\$10,000.00	\$10,000.00		Leave end bump outs and eliminate middle ones
New Curbing	150	LF	\$35.00	\$5,250.00		Reset new curb where bump outs were removed
ADA Ramp Improvements	4	EA	\$1,500.00	\$6,000.00		Provide ADA compliant ramp at all crosswalk and crossing locations with new curb
Re-face Concrete Wall	280	SF	\$150.00	\$42,000.00		Stone veneer on retaining wall in front of Bangor Savings Bank, approx. 70' long x 4' high
Lighting	12	EA	\$3,500.00	\$42,000.00		Replace lights with new fixtures along Main St.
New Wood Fence	50	LF	\$75.00	\$3,750.00		Replace chain-link fence across from Bangor Savings Bank
Plantings	80	EA	\$650.00	\$52,000.00		Street trees from Burnham Tavern to boat launch
Misc. Street Improvements		allow	\$15,000.00	\$15,000.00		Benches, tree grates, etc.
<b>Subtotal</b>					<b>\$161,000.00</b>	
<b>Estimating Contingency (20%)</b>					<b>\$32,200.00</b>	
<b>Main Street Total</b>					<b>\$193,200.00</b>	
<b>Northern Gateway Improvements</b>						
Item	Quantity	Unit	Unit Cost	Item Total	Subtotal	Remarks
Pavement Removals		allow	\$2,500.00	\$2,500.00		Remove striped island at intersection of Main St. and Court St.; remove pavement around existing island to expand landscaped area; maintain traffic pattern
New Curbing	340	LF	\$35.00	\$11,900.00		Curbing around landscaped islands
Loam and Seed	1,500	SF	\$0.60	\$900.00		Loam and seed islands at Main St. and Court St. intersection
Pavement Removals		allow	\$3,800.00	\$3,800.00		Remove parking area across from Helen's Restaurant
Loam and Seed	11,300	SF	\$0.60	\$6,780.00		Loam and seed existing parking area across from Helen's Restaurant
Clearing and Grubbing		allow	\$2,500.00	\$2,500.00		Clear area behind boxcar
Asphalt Paving	80	ton	\$150.00	\$12,000.00		10 car parking area behind boxcar
Loam and Seed	12,100	SF	\$0.60	\$7,260.00		Loam and seed area adjacent to new parking lot
Loam and Seed	38,700	SF	\$0.60	\$23,220.00		Reclaim area adjacent to train station as greenspace
Benches	4	EA	\$600.00	\$2,400.00		
Picnic Tables	4	EA	\$800.00	\$3,200.00		
Trash Receptacles	2	EA	\$300.00	\$600.00		
Signage	1	EA	\$600.00	\$600.00		Sign marking Northern Gateway
Shade Tree Plantings	25	EA	\$650.00	\$16,250.00		Specimen trees
<b>Subtotal</b>					<b>\$93,910.00</b>	
<b>Estimating Contingency (20%)</b>					<b>\$18,782.00</b>	
<b>Northern Gateway Total</b>					<b>\$112,692.00</b>	
<b>Downtown Signage Improvements</b>						
Item	Quantity	Unit	Unit Cost	Item Total	Subtotal	Remarks
Way-finding Interpretive Signage	10	EA	\$250.00	\$2,500.00		Signage identifying public parking and directions to significant buildings
Historical Buildings and Places	5	EA	\$300.00	\$1,500.00		Interpretive signage describing town's history and historic buildings, including Burnham Tavern, Porter Memorial Library, Centre St. Church, Washington County Jail and Court House, and Train Station
<b>Subtotal</b>					<b>\$4,000.00</b>	
<b>Estimating Contingency (20%)</b>					<b>\$800.00</b>	
<b>Signage Total</b>					<b>\$4,800.00</b>	
<b>Pedestrian Bridge Improvements</b>						
Item	Quantity	Unit	Unit Cost	Item Total	Subtotal	Remarks
Bridge Crossing	1	EA	\$450,000.00	\$450,000.00		220' span across Machias River linking trails
<b>Subtotal</b>					<b>\$450,000.00</b>	
<b>Estimating Contingency (20%)</b>					<b>\$90,000.00</b>	
<b>Bridge Total</b>					<b>\$540,000.00</b>	
<b>Project Total</b>					<b>\$1,699,296.00</b>	